



PARKING MANUAL

CITY OF KINGSBURG
1401 DRAPER STREET
KINGSBURG, CA. 93631

September 2008

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PURPOSE

The purpose of this manual is to present the design standards to regulate the development of off-street parking facilities in the City of Kingsburg. The design standards contained herein represent minimum requirements necessary for providing adequately developed parking facilities.

These standards deal with the method of parking vehicles in an off-street facility. In addition, the developer is urged to conduct an independent parking study to determine whether these minimum standards are adequate to meet the parking demands of his specific development.

PARKING DESIGN STANDARDS

1. PARKING SPACE DIMENSIONS

Passenger Vehicle Parking Space

There are two basic passenger vehicle parking space dimensions. 10' x 20' standard painted stalls and 9' x 19' small car (compact) painted stalls. Any parking lot or structure may install these basic stalls in accordance with criteria defined on page 8, Section 3. Should a property owner wish to reduce the two basic stall sizes by constructing continuous concrete curbs the two basic stall sizes may be reduced in size. The four cases defined in Table 1 reflect dimensions for standard and small car stalls, stalls with continuous concrete curbs, and stalls designated for exclusive employee parking areas.

TABLE 1

Case	Length	Width	Requirements
1	20'	10'	Standard painted stall.
2	18'	10'	Standard plus continuous concrete curbs.
3	19'	9'	Compact painted stall.
4	18'	9'	Compact plus continuous concrete curbs.

The conditions that must be met in order to define which basic stall size may be used are stated in Section 3 of this manual, page 8.

Truck Loading Spaces

A truck loading space shall be an accessible rectangle having a width of 12 feet and a length of 40 feet. Any overhead obstruction shall have a vertical clearance of 15 feet.

2. DESIGN CRITERIA

Driveway Approach Construction Standards

Driveway approaches to private property shall be constructed in conformance with the City of Kingsburg Improvement Standards. Special driveway approach designs not shown in the Standards may be approved for developments for which more than 200 parking spaces are provided.

Driveway approaches on public streets may not be used to furnish circulation from one row of parking to an adjacent row of parking. This traffic circulation must be provided on private property.

Backing onto Public Rights-of-Way is Prohibited

A design that requires backing a vehicle onto or from public rights-of-way (public alleys excepted) is prohibited in all areas except residential areas located on streets that are classified as local or collector streets.

Access to Parking Spaces

All parking spaces must have convenient ingress and egress. Access lanes shall be clear and specifically delineated as necessary (except in lots with less than 10 parking stalls). All access to individual parking spaces shall be from access lanes (aisles) within the parking facility or from a public alley. Aisle widths are stated on pages 11 through 14 of this manual. Final approval of aisle widths that are designated as fire lanes shall be by the Kingsburg Fire Department.

Note: An aisle way which provides direct access to parking stalls shall be a one-way aisle, except for parking stalls which are perpendicular or parallel to the aisle way, and where opposing directions are each provided in separate bays (two W-1 bays, see pages 11 through 14) or an opposing aisle is at least 13 feet wide in addition to W-1.

The parking space shall be designed so that the total process of entering and leaving a parking space shall be accomplished in no more movements than two forward and one reverse. No backing maneuver from a parking stall shall conflict or block the public street driveway approach. All such stalls shall be no closer than 20' to the property line at the driveway approach.

With the exception of parking facilities having attendant parking, designing of a parking space so as to require the movement of a vehicle to permit entry or exit from another parking space is not permitted.

All required parking spaces shall be available and accessible at all times for vehicular parking purposes.

Traffic Circulation Signing and Markings

Directional signs and arrows and appropriate pavement marking shall be installed to control the direction of traffic flow, when deemed necessary by the City Engineer.

Surface Requirements

All parking areas shall be surfaced in accordance with the City of Kingsburg Improvement Standards.

Delineation of Parking Spaces

All parking spaces shall be delineated by appropriate fixed curbing, painted lines (a minimum of 4" wide), or other fixed markers. Compact parking stalls or groups of parking stalls shall be individually signed or marked. Any curb painting used to indicate specific use or time limits of parking spaces shall conform to the Municipal Code of the City of Kingsburg.

Delineation of Pedestrian Pathways

All pedestrian pathways necessary to comply with the American with Disability Act and Chapter 11 of the California Building Codes shall be delineated by appropriate fixed curbing, painted lines (a minimum of 4" wide), or other fixed markers.

Physical Barriers

Fixed physical barriers shall be installed to protect public and private property adjacent to the parking facility as well as buildings, landscaping and appurtenances within the development which could be damaged by vehicles using the parking facility. These barriers shall be designed and constructed to facilitate easy cleaning of the parking surface.

A solid masonry wall shall be constructed when required by the zoning ordinance.

When no masonry wall is required, wheel stops in the form of a 6" high concrete curb or other approved fixed barrier, placed a minimum distance of 3' from the property line, or the building to be protected, shall be installed. Landscaping shall be adequately protected to avoid damage by vehicles.

Generally, the fixed physical barrier will be placed 3 feet from the property line, or the building to be protected; however, this distance must be a minimum of 5 feet if the vehicles are permitted to back into the parking stalls.

Landscaping

Landscaping and irrigation systems shall be installed where required by the Zoning Ordinance or other condition to zoning.

For those parking facilities where landscaping is not required, the developer is encouraged to install landscaping to improve the appearance of his premises and of the general neighborhood.

Lighting

A lighting system shall be installed on all off-street parking areas. This lighting system shall be designed to produce a minimum maintained average light level of one-half (1/2) foot candle on the entire parking facility's horizontal surface, including the parking spaces, the loading spaces and the vehicular and pedestrian circulation areas.

The system shall have a maximum brightness ratio of 6 to 1. The lighting fixtures shall be hooded and so arranged and controlled as not to cause a nuisance either to highway traffic or adjacent properties. When the parking facility is open to the public during darkness, this lighting system shall be operating sufficiently to produce the required minimum of one-half (1/2) foot-candle. All lighting designs are subject to review and approval by the City Engineer or Building Official.

Maintenance of Parking Facilities and Equipment

All paving, directional devices and protective equipment, landscaping, and other equipment furnished or required on the parking facility shall be maintained to insure safe pedestrian movement, vehicular operation, adequate protection of adjoining properties, and to present a neat and attractive appearance of the facility.

Parking Lot Design

The design of parking lots shall conform to the minimum standards shown on pages 11 through 14.

3. CRITERIA TO DETERMINE STALL SIZE

- A. Any parking lot or structure may use Case #1 stall dimensions for design.
- B. In order to use Case #2 dimensions a 6" high continuous concrete curb must be used. Concrete wheel stops are not an acceptable substitute. The allowable vehicle overhang is 2'.
- C. Case #3 reflects the basic size for compact car spaces in lots or structures. 9' x 19' is the basic dimension. Up to five percent (5%) of the parking spaces in any given parking lot in the commercial, industrial, manufacturing, school, or hospital zone districts may be designated for compact cars with the approval of the Planning Commission. Residential uses may not use compact car spaces. Before any small car standards are allowed 19 standard size stalls must be provided. These 19 standard size stalls must be located on the site closest to the main building entrance.
- D. Case #4 dimensions may be used with the same criteria stated in Case #3 with the addition of a 6" high continuous concrete curb. Concrete wheel stops are not an acceptable substitute. The allowable front vehicle overhang is 1'.

4. HANDICAP PARKING STALLS

Handicap Parking Stalls shall be provided as required in the zoning ordinance. The design of such stalls shall be in accordance with Chapter 11 of the California Building Code.

5. PERFORMANCE STANDARDS FOR PARKING LOT SHADING **POLICY**

Fifty percent of paved parking lots surface shall be shaded by tree canopies within fifteen years of planting.

GENERAL

- a. A "paved parking lot" shall include parking stalls, driveways, and maneuvering areas.
- b. Trees planted to satisfy the requirements of these guidelines are subject to established landscaping requirements.

SITE PLAN REQUIRED

- a. A landscape plan which details the degree of compliance with the shading requirements is required. The plan shall show:
 - a. All landscaped areas.
 - b. Tree canopies drawn to scale representing the estimated canopy at a fifteen year growth period.
 - c. The total area in square feet of the paved parking lot, driveways, and maneuver areas; and the area shaded by tree canopies. A schedule listing total parking area, shaded area, and the percentage of parking area shaded should be included.
 - d. A schedule of the specific names of proposed trees and their sizes.
 - e. Such plan shall be approved by the Planning and Community Development Department prior to issuance of building permits. However, the plan is encouraged to be submitted at the time of site plan review.

METHODOLOGY

- a. To simplify the process of determining compliance, the true angle of deflection of natural sunlight shall not be considered. Shaded areas shall be assumed to be only those portions of a paved parking lot directly beneath the shading canopy or drip line.
- b. Shading shall be provided by tree canopies except that any portion of a paved parking lot directly beneath and shaded by a man made structure (overhangs and covered parking, for instance) shall be deleted from the requirements of these performance standards and shall be subtracted from the area of the parking lot to be shaded.
- c. Credit shall be given only for surface area shaded. Multiple canopies shading the same surface area will not be counted as multiple credit.
- d. Landscape planters beneath the canopy may be considered as shaded parking areas for the purpose of determining compliance.
- e. Where more than five trees are to be planted, mix tree species.
- f. Trees planted along the perimeter of a lot may be counted as providing shade for the full area of their canopy.
- g. If the degree of overlap between trees is less than 15 percent, all trees may be counted as shading 100 percent of their canopy. If the degree of overlap is 15 percent or greater, then it will be necessary to perform individual

calculation to determine the area of shading.

- h. A ten percent minor deviation of the shading standard may be approved by the Planning & Community Development Director in accordance with established procedures in the Municipal Code if it is found that the normal standards would impose an undue hardship.

MINIMUM TREE REQUIREMENT

Provide one medium size tree for every two required parking spaces. (This requirement may be modified by the Planning and Community Development Director if the standards for shading and perimeter planting have been met.)

Disperse trees over the parking lot area to provide 50 percent shading of the parking area surface within 15 years (This requirement may be reduced to 40 percent for existing development if it is demonstrated that the constraints of an existing site would make it impossible to meet the normal standard). Trees shall also be planted in the required landscaped areas along the periphery of the development in order to shade and enhance adjacent property and public rights-of-ways. Trees shall be maintained in good health. However, trees may not be trimmed or pruned to reduce the natural height or overall crown of the tree, except as necessary for health of the tree and public safety; or as may otherwise be approved by the Planning and Community Development Department.

MINIMUM PLANTER SIZE (between tiers of parking)

Continuous Planter

Standard Parking Stalls 8 feet

Compact Parking Stalls 6 feet

Along Periphery 10 feet except as may be approved by the Planning & Community Development Director.

MINIMUM SIGHT TRIANGLE

10 feet in traffic areas

30 – 35 feet at street intersections

100 feet at major street intersections

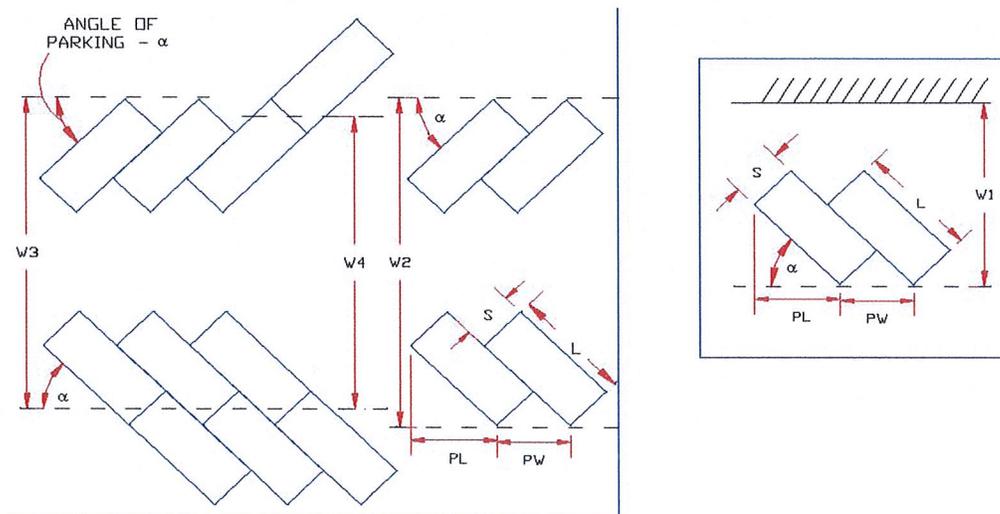
Trees within the triangle shall be trimmed up to eight-feet minimum above the surface of the parking lot.

Other landscaping (ground cover and shrubs) shall not exceed two and one-half feet above the parking lot surface.

6. TABLE OF DESIGN DIMENSION FOR VARIOUS PARKING ANGLES

TABLE OF DESIGN DIMENSION FOR VARIOUS PARKING ANGLES - CASE 1

ANGLE OF PARKING	PARKING BY DIMENSIONS				AISLE WIDTH	DESIGN LAYOUT DIMENSIONS	
	W-1	W-2	W-3	W-4		P.L.	P.W.
30.0	29.5	46.5	42.5	39.0	12.0	29.9	19.0
35.0	30.5	48.5	45.0	41.0	12.0	26.1	16.7
40.0	31.0	50.0	47.0	43.5	12.0	22.8	15.0
45.0	32.0	51.5	48.5	45.0	12.0	19.8	13.7
50.0	33.5	53.5	51.0	48.0	13.0	17.1	12.8
55.0	33.5	54.5	52.0	49.5	13.0	14.5	12.0
60.0	35.0	56.0	53.5	51.5	14.0	12.1	11.4
65.0	37.0	58.0	56.0	54.0	16.0	9.8	10.9
70.0	39.0	60.0	58.5	57.0	18.0	7.6	10.6
75.0	40.5	61.5	60.0	59.0	20.0	5.5	10.3
80.0	43.5	63.5	63.0	62.0	23.0	3.6	10.1
85.0	44.5	64.5	64.0	63.5	25.0	1.7	10.0
90.0	47.0	65.0	65.0	65.0	27.0	0.0	10.0



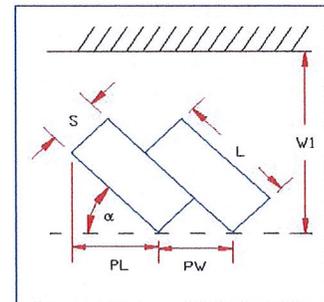
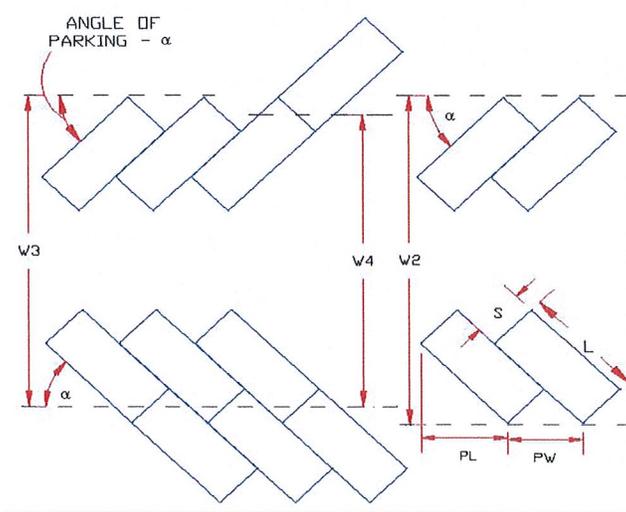
S = 10.0 ft.

L = 20.0 ft.

Note: An aisle way which provides direct access to parking stalls shall be a one-way aisle, except for parking stalls which are perpendicular or parallel to the aisle way

TABLE OF DESIGN DIMENSION FOR VARIOUS PARKING ANGLES - CASE 2

ANGLE OF PARKING	PARKING BY DIMENSIONS				AISLE WIDTH	DESIGN LAYOUT DIMENSIONS	
	W-1	W-2	W-3	W-4		P.L.	P.W.
30.0	29.0	45.5	41.5	38.0	12.0	29.1	19.0
35.0	29.5	47.5	43.5	40.0	12.0	25.3	16.7
40.0	30.5	49.0	45.5	42.0	12.0	22.0	15.0
45.0	31.0	50.0	47.0	44.0	12.0	19.1	13.7
50.0	32.5	52.0	49.5	46.5	13.0	16.4	12.8
55.0	33.0	53.0	50.0	47.5	13.0	13.9	12.0
60.0	34.0	54.0	52.0	49.5	14.0	11.6	11.4
65.0	36.0	56.0	54.5	52.5	16.0	9.4	10.9
70.0	38.0	58.0	56.5	55.0	18.0	7.3	10.6
75.0	39.5	59.5	58.5	57.0	20.0	5.3	10.3
80.0	42.5	61.5	61.0	60.0	23.0	3.4	10.1
85.0	43.5	62.5	62.0	61.5	25.0	1.6	10.0
90.0	46.0	63.0	63.0	63.0	27.0	0.0	10.0



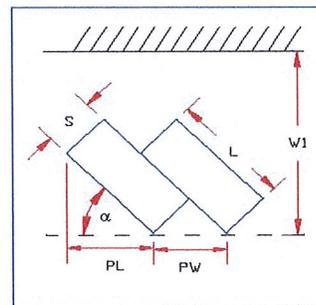
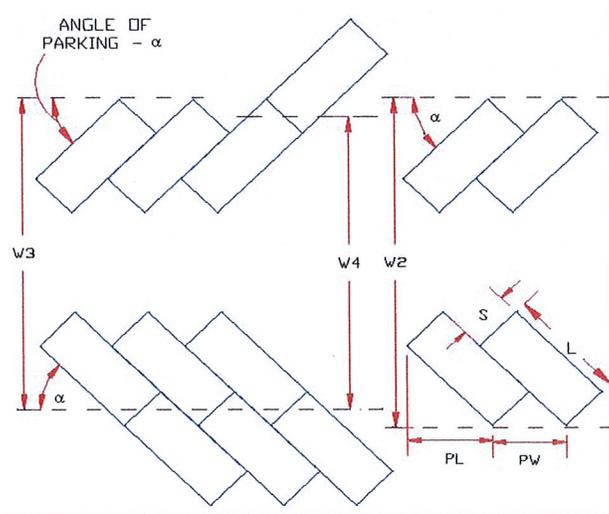
$S = 10.0$ ft.

$L = 18.0$ ft.

Note: An aisle way which provides direct access to parking stalls shall be a one-way aisle, except for parking stalls which are perpendicular or parallel to the aisle way.

TABLE OF DESIGN DIMENSION FOR VARIOUS PARKING ANGLES - CASE 3

ANGLE OF PARKING	PARKING BY DIMENSIONS				AISLE WIDTH	DESIGN LAYOUT DIMENSIONS	
	W-1	W-2	W-3	W-4		P.L.	P.W.
30.0	29.5	46.5	42.5	39.0	12.0	29.9	18.0
35.0	30.5	48.5	45.0	41.0	12.0	26.1	15.7
40.0	31.0	50.0	47.0	43.5	12.0	22.8	14.0
45.0	32.0	51.5	48.5	45.0	12.0	19.8	12.7
50.0	33.5	53.5	51.0	48.0	13.0	17.1	11.8
55.0	33.5	54.5	52.0	49.5	13.0	14.5	11.0
60.0	35.0	56.0	53.5	51.5	14.0	12.1	10.4
65.0	37.0	58.0	56.0	54.0	16.0	9.8	9.9
70.0	39.0	60.0	58.5	57.0	18.0	7.6	9.6
75.0	40.5	61.5	60.0	59.0	20.0	5.5	9.3
80.0	43.5	63.5	63.0	62.0	23.0	3.6	9.1
85.0	44.5	64.5	64.0	63.5	25.0	1.7	9.0
90.0	47.0	65.0	65.0	65.0	27.0	0.0	9.0

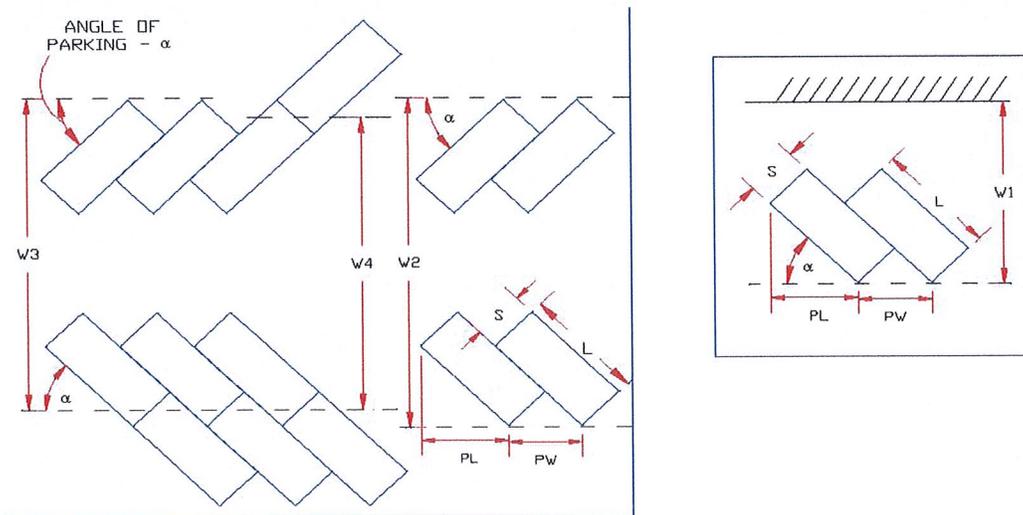


S = 9.0 ft.
L = 19.0 ft.

Note: An aisle way which provides direct access to parking stalls shall be a one-way aisle, except for parking stalls which are perpendicular or parallel to the aisle way

TABLE OF DESIGN DIMENSION FOR VARIOUS PARKING ANGLES - CASE 4

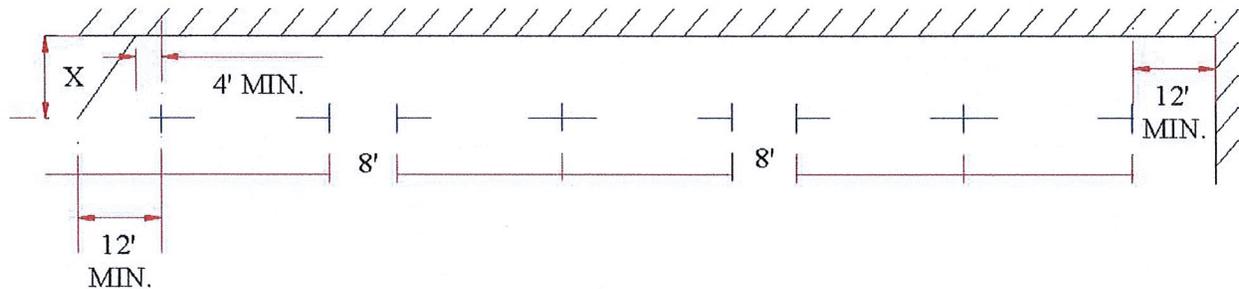
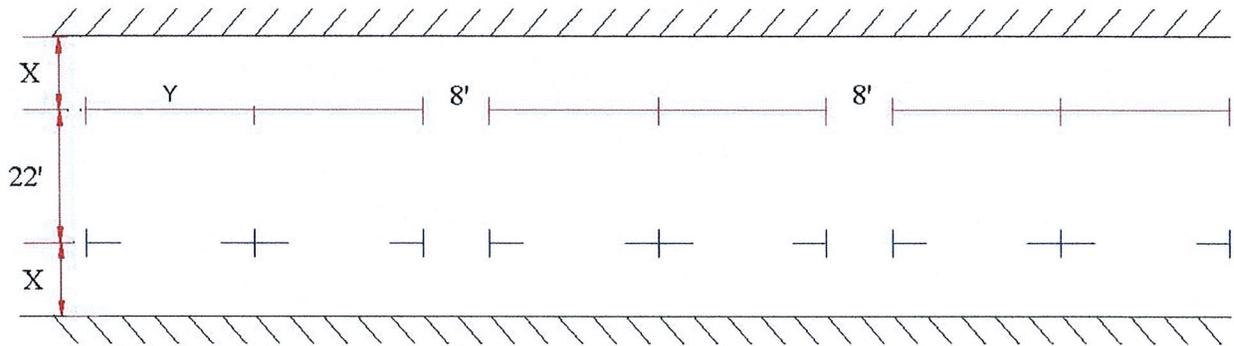
ANGLE OF PARKING	PARKING BY DIMENSIONS				AISLE WIDTH	DESIGN LAYOUT DIMENSIONS	
	W-1	W-2	W-3	W-4		P.L.	P.W.
30.0	29.0	45.5	41.5	38.0	12.0	29.1	18.0
35.0	29.5	47.5	43.5	40.0	12.0	25.3	15.7
40.0	30.5	49.0	45.5	42.0	12.0	22.0	14.0
45.0	31.0	50.0	47.0	44.0	12.0	19.1	12.7
50.0	32.5	52.0	49.5	46.5	13.0	16.4	11.8
55.0	33.0	53.0	50.0	47.5	13.0	13.9	11.0
60.0	34.0	54.0	52.0	49.5	14.0	11.6	10.4
65.0	36.0	56.0	54.5	52.5	16.0	9.4	9.9
70.0	38.0	58.0	56.5	55.0	18.0	7.3	9.6
75.0	39.5	59.5	58.5	57.0	20.0	5.3	9.3
80.0	42.5	61.5	61.0	60.0	23.0	3.4	9.1
85.0	43.5	62.5	62.0	61.5	25.0	1.6	9.0
90.0	46.0	63.0	63.0	63.0	27.0	0.0	9.0



S = 9.0 ft.
L = 18.0 ft.

Note: An aisle way which provides direct access to parking stalls shall be a one-way aisle, except for parking stalls which are perpendicular or parallel to the aisle way.

6. PARALLEL PARKING DESIGN DIMENSIONS (STANDARD STALLS)



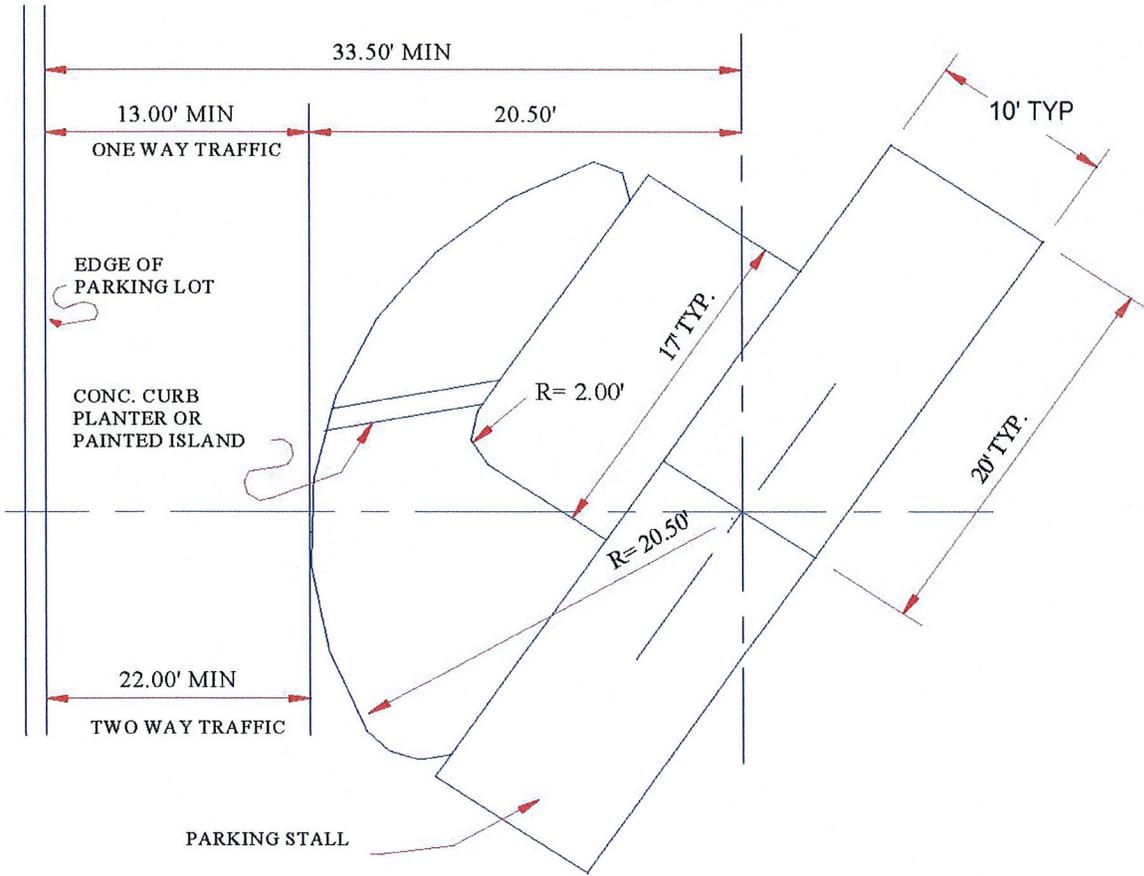
X = 10' IF ADJACENT TO AN OBSTRUCTION OVER 8" HIGH SUCH AS A WALL OR FENCE.

X = 8' IF ADJACENT TO A SIDEWALK OR SIMILAR BARRIER LESS THAN 8" HIGH AND AT LEAST 2' WIDE.

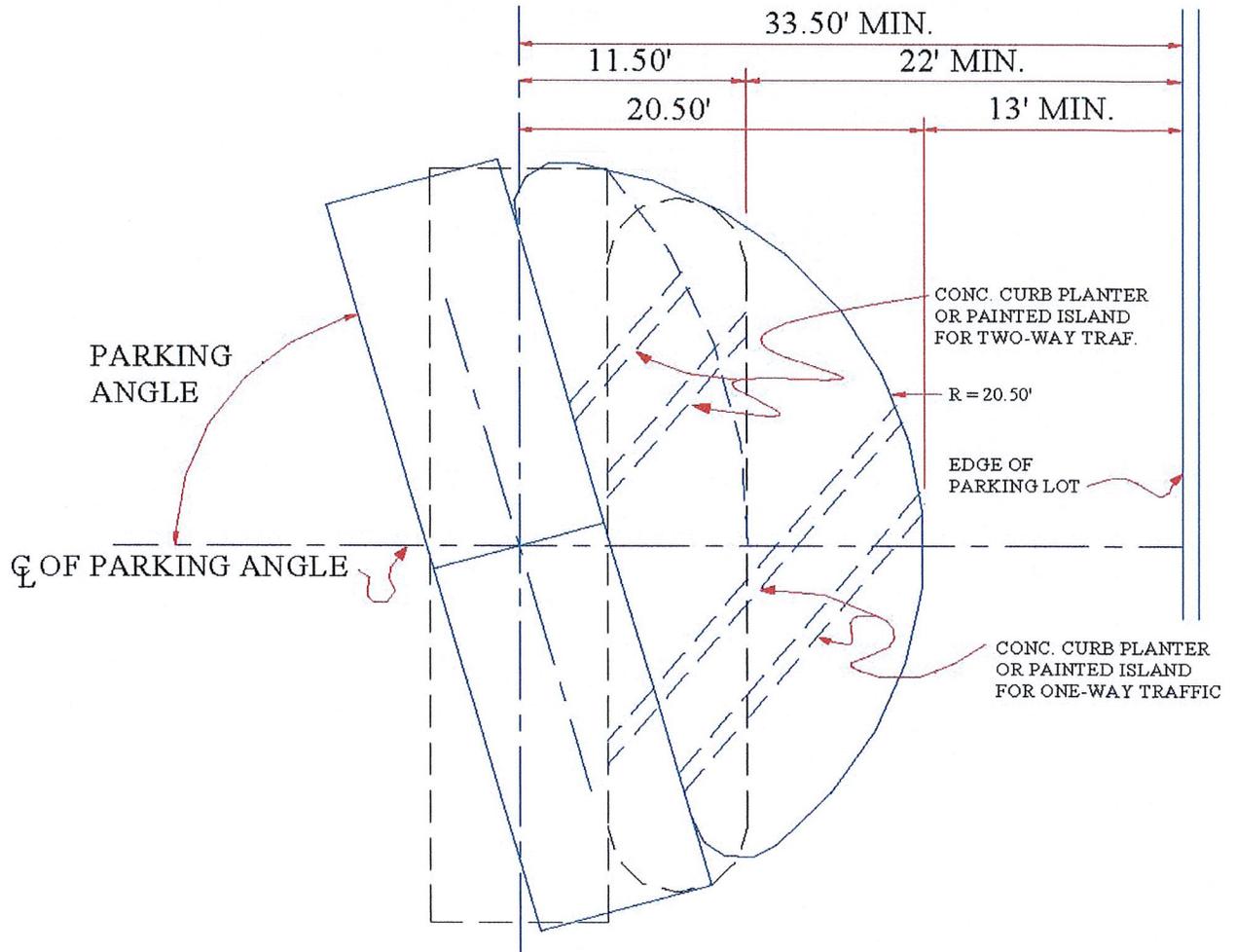
NOTE: MINIMUM ONE-WAY AISLE - 13'
MINIMUM TWO-WAY AISLE - 22'

Y = 20' UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER

FULL-TURN DETAIL FOR 65° PARKING ANGLE OR LESS



FULL-TURN DETAIL FOR MORE THAN 65 PARKING (STANDARD STALLS)



RESOLUTION NO. 2008-51

**A RESOLUTION OF THE CITY COUNCIL
OF THE CITY OF KINGSBURG APPROVING
PARKING STANDARDS**

WHEREAS, the applicant, City of Kingsburg, has identified the need for parking standards, and;

WHEREAS, staff has prepared the City of Kingsburg Parking Standards that addresses the minimum standards for parking lot facilities within the City of Kingsburg, and;

WHEREAS, the parking manual is consistent with adopted City standards and plans, and;

WHEREAS, The Planning Commission of the City of Kingsburg recommended approval of the City of Kingsburg Parking Manual and its use in setting minimum standards for parking lot construction within the city.

NOW, THEREFORE BE IT RESOLVED that:

The City Council of the City of Kingsburg hereby approves the City of Kingsburg Parking Manual and its use in setting minimum standards for parking lot construction within the city.

I, Sue Bauch, City Clerk, do hereby certify that the foregoing resolution was duly passed and adopted at a regular meeting of the Kingsburg City Council held on the 1st day of October 2008, by the following vote:

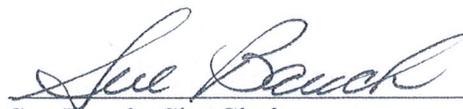
Ayes:	Council Member(s):	Blayney, Kruper, Karstetter, Smith, and Mayor Bergstrom
Noes:	Council Member(s):	None
Absent:	Council Member(s):	None
Abstain:	Council Member(s):	None

CITY CLERK'S CERTIFICATION

I, Susan Bauch, City Clerk of the City of Kingsburg, hereby certify that the foregoing is a true and correct copy of the complete original thereof on file with the City of Kingsburg.

10/2/08
Date


Susan Bauch, City Clerk
City of Kingsburg


Sue Bauch, City Clerk
City of Kingsburg